

# WASHINGTON NATIONAL TRANSIT ORIENTED DEVELOPMENT DISTRICT STREETSCAPE PLAN

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AHBE



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## INTRODUCTION

The purpose of this *Washington National Transit Oriented Development District Streetscape Plan* is to create a series of principles to guide the streetscape design in the vicinity of the Culver City Expo Line Station, and within an emerging Transit Oriented Development (TOD) district.

Spurred by the arrival of the Exposition Light Rail Line (Expo) from Downtown Los Angeles to Culver City in Spring of 2012, a series of new TODs are being planned and constructed near the intersection of Washington and National Boulevards. Phase II Expo Line is currently being extended to Santa Monica with service anticipated to commence in 2016.

These projects include *Access Culver City*, a 115-unit mixed use development by Greystar Real Estate; *Platform* at Culver Station by the Runyon Group; and *The Ivy Station* by Lowe Enterprise Real Estate Group adjacent to the Expo Culver City station. These new developments require common area public improvements to form a cohesive and attractive pedestrian environment within the *Washington National Transit Oriented Development District*.

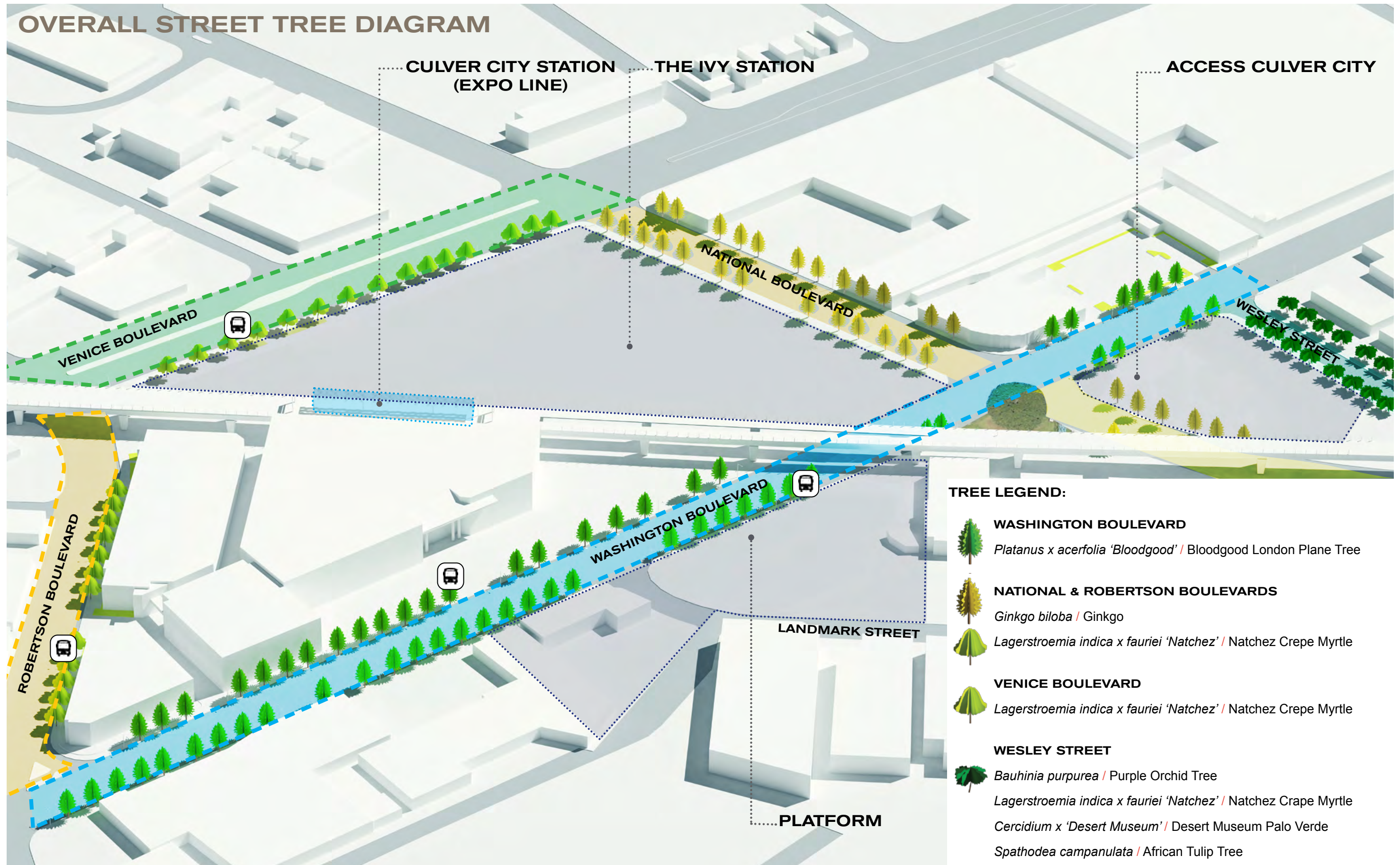
The plan is intended to promote area revitalization through implementation of pedestrian friendly streetscape enhancements including canopy street trees, street furniture, graphics, new crosswalk paving and Low Impact Development (LID) features such as bioswales and filtration planters.

The plan comports with the *Bicycle Pedestrian Master Plan* that was approved by the City Council in 2010 and promotes multi-mobility, connectivity, and sustainability.





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# COMMON DESIGN ELEMENTS



"Towne Square" Bench by Landscape Forms



Silva Cells by Deep Root



4" x 24" Concrete Unit Plank Pavers



4" x 8" Permeable Concrete Unit Pavers



"Scarborough" Trash / Recyclables Receptacle by Landscape Forms



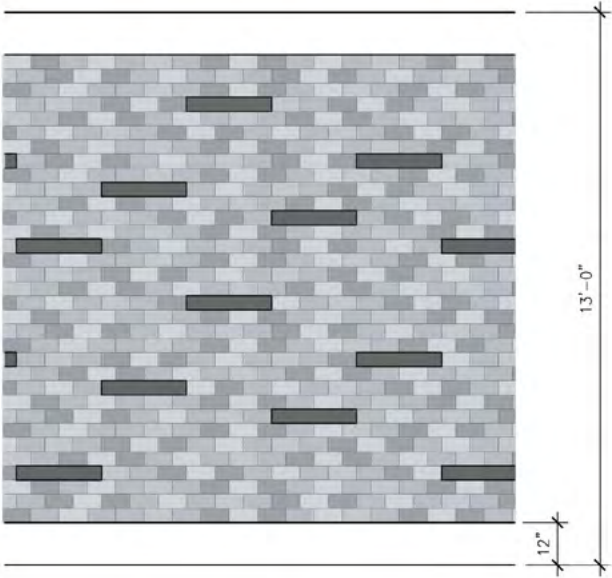
"Market Street" Tree Grate by Ironsmith



"Simple-Lok" by Sunshine U-LOK Corporation



Concrete Pavement (Natural Gray Top-Cast, Finish #5)



Standard Paver Running Bond Pattern

In order to visually unify the *Washington National Transit Oriented Development District*, consistent paving, street furniture, graphic signage, and LID technologies will be used throughout.

Canopy shade trees will be located at regular spacing along the streetscape. Intermittent bands of low planting will occur between the street trees.

Paving will be primarily a natural color concrete with a surface finish (*Top-Cast* by Dayton Superior). Bands and fields of *Aqua-via* (City Blend mix) concrete paver units and 4x16 (Charcoal) linear paving stones by Acker-Stone will distinguish the LID features and add a rich character to unify the district; 4x8 unit pavers will distinguish the crosswalks in a running bond pattern.

A new interpretive signage/environmental graphics program with the Culver City logo will provide information and way-finding and unify the district. This will consist primarily of district banners on existing light poles, way-finding signs and directories. Per TOD stakeholders' input, it is recommended that the transit district be referred to as *The Hub*. Proposed graphics have been developed to highlight and promote multi-mobility within the district.

At designated areas, seat nodes will be located adjacent to the street trees. The standard bench to be used throughout the district is the *Town Square* bench by Landscape Forms (49" length with interim divider to discourage sleeping). Accompanying the benches will be *Scarborough* trash/recyclables receptacles by Landscape Forms (24" diameter) and a pair of bike racks (*Simple-Lok* by Sunshine U-Lok Corporation).

The primary LID features will be concrete filtration planters and structural soil systems. The planters will filter storm water via a soil medium prior to discharge into the city storm water system. (Note:

the accompanying geotechnical investigation confirms that *infiltration* of storm water is not recommended.)

Decorative lighting, such as colored or patterned lighting of the columns and underside of the bridge of the Expo Line overpass, will highlight this distinct architectural feature.

The structured soil system (*Silva Cell* by Deep Root) provides for planting soil cells under the sidewalk and adjacent to the street trees to allow for greater root volume and therefore larger tree canopies.

Street trees at the filtration planter will have tree grates (*Market Street* by Ironsmith and *Grate Stakes* by JR Partners). The tree grates will be finished with a rust inhibitor product (i.e. *Black Max*) to expedite to oxidation process and mitigate corrosion. Tree grate openings can be cut to make a larger area for the trunk as the trees mature.